

REPORT FROM: PLANNING, BUILDING CONTROL AND LICENSING SERVICES MANAGER

TO: BRIERFIELD AND REEDLEY COMMITTEE

DATE: 4th September 2018

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PLANNING APPLICATIONS

PURPOSE OF REPORT

To determine the attached planning applications

REPORT TO BRIERFIELD AND REEDLEY COMMITTEE ON 4TH SEPTEMBER 2018

Application Ref: 18/0430/FUL
Proposal: Full: Erection of replacement dwelling and associated car parking.
At: 9 Ainsdale Avenue, Reedley
On behalf of: Mr A Hussain
Date Registered: 17.07.2018
Expiry Date: 11.09.2018
Case Officer: Charlotte Pinch

Site Description and Proposal

The application is brought to Committee as it received more than three objections.

The application site is a detached bungalow located within the south east of the settlement boundary of Brierfield. It is located on Ainsdale Avenue which is a residential cul-de-sac with properties of varied styles and frontages. The site has a large area of open land to the front, with extensive parking areas.

The proposal is for the erection of a replacement dwelling and associated car parking. The development would result in a two storey dwelling, with four additional bedrooms and three bathrooms at first floor level. The dwelling would feature a significant two storey rear extension and be predominantly constructed of render, aluminium powder coated fenestration and blue grey slate roof tiles.

This proposal in principle is acceptable; however amended plans have been requested to address design and residential amenity concerns. These include:

- Removal of the front and side balconies.
- Alteration to the two storey flat roof front extension.
- Regularise the gable feature on the front elevation and windows reduced in size.
- Reduce the depth of the two storey rear projection, to ensure no loss of light to No.20 Larkhill Avenue.

Relevant Planning History

13/15/0485P

Full: Erection of two dwellings (one pair semi-detached) with associated curtilage, parking with access from Larkhill Avenue (re-submission).

Approved with Conditions. 2015.

17/0396/HHO

Full: Erection of two storey extension to rear, installation of dormers on the front, rear and both side roof slopes and alterations to the roof.

Approved with Conditions. 2017.

Consultee Response

LCC Highways

Having considered the information submitted for the above application, and observations on site today (10.8.18), the Highway Development Support Section does not have any objections regarding the proposed development at the above location, subject to the following comments being noted, and conditions being applied to any formal planning approval granted.

Parking provision

Based on the recommendations in the 'Replacement Pendle Local Plan 2001-2016 Appendix 2: Car and Cycle Parking Standards' in our opinion the applicant has provided adequate off-road parking provision for this type and size of development.

Site access

Ainsdale Avenue is a privately maintained road, that is, not maintained at the public expense. There is a large, gravelled area at the end of Ainsdale Avenue outside the entrance to No 9 which appears to be maintained by other residents on the avenue. As the only access to No 9 is across part of this gravelled area we are concerned that construction vehicles using it may cause a deterioration in its condition. Therefore we ask for a pre-commencement condition survey to be carried out.

Given the development proposed, together with the limited access to the site, there could be difficulties with construction traffic during both the demolition and construction phases. This would need to be carefully managed so as not to obstruct access to, nor have a detrimental effect on, neighbouring properties. The applicant would, therefore, need to submit a construction method statement prior to the commencement of any works, for the reasons given above.

Furthermore, due to the site's location within a residential area, and close to Reedley Primary School on Reedley Road, we recommend that a condition is applied restricting the times of deliveries to ensure there is no conflict with traffic, both vehicular and pedestrian, at peak times in and out of the estate.

United Utilities

In accordance with the National Planning Policy Framework (NPPF) and the National Planning Practice Guidance (NPPG), the site should be drained on a separate system with foul water draining to the public sewer and surface water draining in the most sustainable way. The NPPG clearly outlines the hierarchy to be investigated by the developer when considering a surface water drainage strategy.

If the applicant intends to obtain a water supply from United Utilities for the proposed development, we strongly recommend they engage with us at the earliest opportunity.

Public Response

Six letters of objection were received from neighbouring occupiers, their comments can be summarised as follows:

- Use of Ainsdale Avenue for construction vehicles could damage the road and sewer pipes below.
- The use of Larkhill Avenue for construction traffic would be preferable.

- No pavement on Ainsdale Avenue, therefore construction traffic would pose a risk to pedestrians.
- Loss of parking and turning area on Larkhill Avenue if the access were to be altered.
- Significant increase in the floor area of the property, two storey structure would not be in keeping with adjacent bungalows.
- Proposed balconies would result in overlooking to neighbouring properties and loss of privacy.

Officer Comments

Policy

Pendle Local Plan Part 1: Core Strategy

Policy ENV1 (Protecting and Enhancing Our Natural and Historic Environments) of the Pendle Local Plan Part 1 seeks to ensure a particularly high design standard that preserves or enhances the character and appearance of the area and its setting. It states that the impact of new developments on the natural environment, including biodiversity, should be kept to a minimum.

Policy ENV2 (Achieving Quality in Design and Conservation) of the Pendle Local Plan Part 1 identifies the need to protect and enhance the heritage and character of the Borough and quality of life for its residents by encouraging high standards of quality and design in new development. It states that siting and design should be in scale and harmony with its surroundings.

Replacement Pendle Local Plan

Saved Policy 31 of the Replacement Pendle Local Plan sets out the maximum parking standards for development.

National Planning Policy Framework

The Framework states that the purpose of the planning system is to contribute to the achievement of sustainable development. It states that there are three dimensions to sustainable development: economic, social and environmental. The policies within the Framework, taken as a whole, constitute the Government's view of what sustainable development in England means in practice for the planning system.

Paragraph 130 states that permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

The Design Principles Supplementary Planning Document (SPD) applies to householder extensions and sets out the aspects required for good design.

Design

Policy ENV2 requires high standards of quality and design in new development to ensure protection of the character of the borough and quality of life for local residents.

Subject to the receipt of satisfactory amended plans, the forward projections proposed on the front elevation of the replacement dwelling will present no unreasonable impacts on the street scene of the area, given the variation of property styles and frontages in the vicinity.

The pitched roof features of the dwelling are in keeping with the prominent gable features in the street scene.

Materials to complement the existing dwellinghouse are proposed, these comprise of smooth render, blue/grey slate roof tiles and grey powder coated aluminium windows.

The design, scale and materials of this development are acceptable and therefore comply with Policy ENV2, Paragraph 130 of the NPPF and the Design Principles SPD.

Residential Amenity

The Design Principles SPD states that developments must adequately protect neighbours enjoying their own home and should take account of regular spacing between buildings. Extensions must not overshadow to an unacceptable degree or have an overbearing impact on neighbouring properties.

The proposed replacement dwelling would be sited 1 metre from the northern side boundary with No.20 Larkhill Avenue and 2 metres from the southern side boundary with No.7 Ainsdale Avenue. In accordance with the SPD these are adequate separation distances to ensure the development does not appear overbearing to neighbouring occupiers.

Provided amended plans show a reduction in the depth of the two storey rear projection from 9.1 metres to 8 metres the proposal would not cause an unacceptable loss of light to either No.20 Larkhill Avenue or No.7 Ainsdale Avenue.

The SPD further guides that windows in extensions should not directly or inappropriately overlook adjacent properties.

The proposed dwelling shows four first floor obscure glazed windows on the northern side elevation of the property. A condition should be applied to any permission to ensure these remain obscure glazed, to prevent any detrimental overlooking.

The south side elevation of the two storey rear projection looks towards the rear garden of No.7 Ainsdale Avenue. Given the proposed 1.8 metre privacy screen and 8 metre separation distance from the south side boundary, a small balcony and patio doors in this location would not result in a significant loss of privacy to neighbouring occupiers.

Therefore, the proposed development is acceptable in terms of residential amenity in accordance with Policy ENV2 and the Design Principles SPD.

Highways

No objections have been raised by LCC Highways, with regards to highway safety and I concur with that view.

The proposed development would form a five bedroom dwelling, therefore three on plot parking spaces would be required. A large driveway is shown to the front of the dwelling, accessed off Ainsdale Avenue, with three parking spaces at the head of the driveway. This would be sufficient parking provision and retain adequate space for manoeuvring.

Reason for Decision

Section 38 of the Planning and Compulsory Purchase Act 2004 requires that applications be determined in accordance with the development plan unless material considerations indicate otherwise. The proposed residential development would accord with Local Planning Policy and would be compliant with the guidance set out in the Framework. The development therefore complies with the development plan. There is a positive presumption in favour of approving the development and there are no material reasons to object to the application.

RECOMMENDATION: Delegate Grant Consent Subject to Acceptable Amendments to the Design Being Received.

Subject to the following conditions:

1. The proposed development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: Required to be imposed by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans: Drawing No: Dwg 06, Dwg 07, Dwg 04 and Dwg 03.

Reason: For the avoidance of doubt and in the interests of proper planning.

3. Prior to the commencement of development samples of all the surfacing materials and external materials of the dwelling to be used in the construction of the roof, walls, boundary/retaining walls and rainwater goods together with samples of the colour and finish of windows and doors of the development hereby approved, shall be submitted to and approved in writing by the Local Planning Authority. The development shall thereafter at all times be carried out in strict accordance with the approved materials and details.

Reason: To ensure a satisfactory form of development in the interest of visual amenity of the area and to respect the character of the conservation area.

4. Prior to the first use of the dwelling hereby approved, the car parking spaces within the curtilage of the dwelling shall be surfaced and laid out in accordance with drawing HUSSAI/01-Dwg04. These areas shall remain available for parking thereafter.

Reason: To ensure suitable parking provision within the site.

5. Before the dwelling hereby permitted is occupied, the access to be used for vehicular purposes extending from the highway boundary of a minimum distance of 5 metres into the site shall be appropriately paved in tarmacadam, block pavements or other approved material.

Reason: In the interests of highway safety.

6. No development shall take place, including any works of clearance, until a construction code-of-practice method statement has been submitted to and approved in writing by the Local Planning Authority. The approved statement shall be adhered to throughout the construction period. It shall provide for:

- i) The parking of vehicles of site operatives and visitors
- ii) The loading and unloading of plant and materials
- iii) The storage of plant and materials used in constructing the development
- iv) Wheel washing facilities
- v) Measures to control the emission of dust and dirt during construction
- vi) Details of working hours
- vii) Routing of delivery vehicles to/from site.

The construction code-of-practice should be compiled in a coherent and integrated document and must be accessible to the site manager(s), all contractors and sub-contractors working on site. As a single point of reference for site environment management, the CCP should incorporate all agreed method statement. All works agreed as part of the plan shall be implemented during an agreed timescale.

Reason: To protect existing road users and the amenity of neighbouring residents.

7. Foul and surface water shall be drained on separate systems. Surface water shall be drained in accordance with the hierarchy of drainage options in national planning practice guidance. In the event of surface water discharging to public sewer, the rate of discharge shall be restricted to the lowest possible rate which shall be agreed with the statutory undertaker prior to connection to the public sewer.

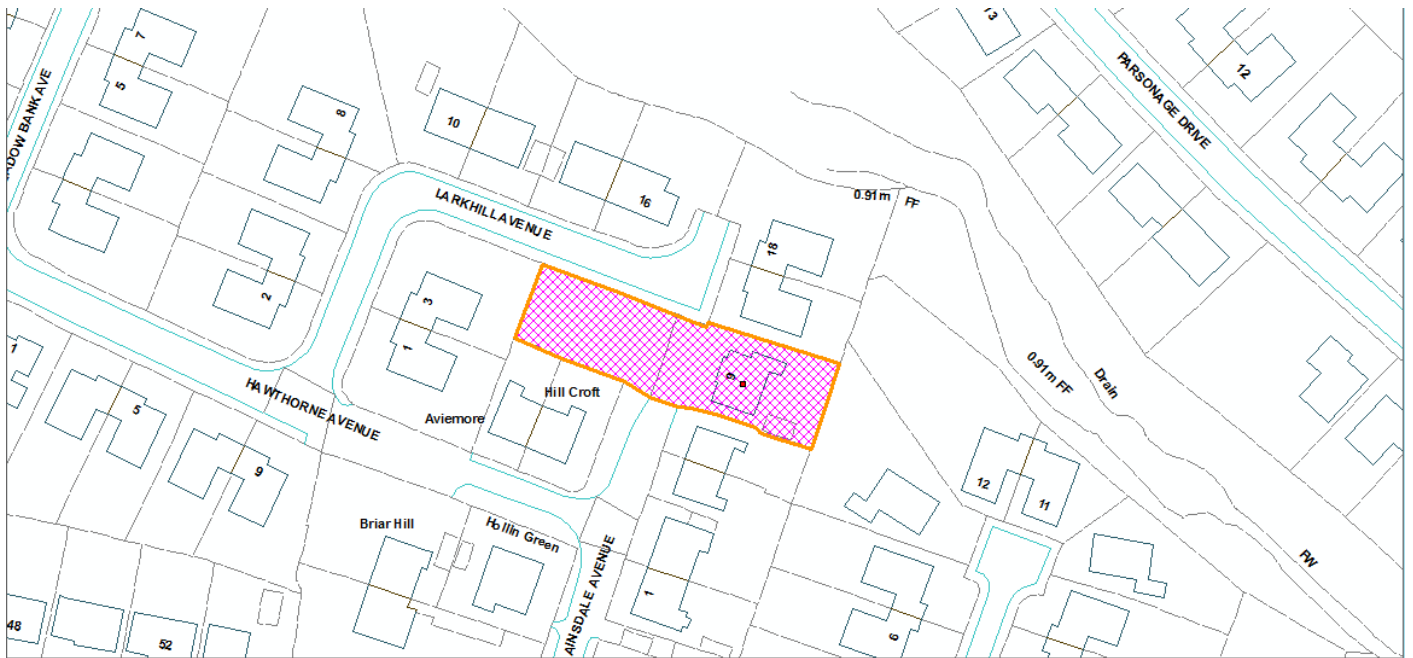
Reason: To secure proper drainage and to manage the risk of flooding and pollution.

8. The first floor window, serving a bathroom on the north side elevation of dwelling 2 and first floor window on the south side elevation of dwelling 3 the development hereby permitted shall at all time be glazed only with obscure glass and be retained as such. As a replacement glazing shall be of an equal degree of obscurity. The window shall also be hung in such a way so as to prevent the effect of obscure glazing being negated by way of opening.

Reason: In order to protect and preserve the privacy and amenity of the occupiers of the adjoining dwelling.

9. Prior to occupation of the proposed dwelling, full details of balcony privacy screens shall be submitted to and approved in writing by the Local Planning Authority. Such screens shall be erected before the dwelling hereby approved is first occupied and retained permanently thereafter.

Reason: In the interests of amenity and privacy.



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Proposal: Full: Erection of replacement dwelling and associated car parking.

At: 9 Ainsdale Avenue, Reedley

On behalf of: Mr A Hussain

LIST OF BACKGROUND PAPERS

Planning Applications

NW/MP

Date: 22nd August 2018